

DISPATCH



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NASG DISPATCH
Official Publication of the
National Association of S Gaugers

The NASG **DISPATCH** welcomes art, photographs, letters, articles and other S gauge/scale related materials contributed by the membership. Send all such materials to the editor.

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Membership year is determined by the date you sign up guaranteeing 6 issues starting with the next issue after signup. All applications, renewals and membership questions should be directed to:

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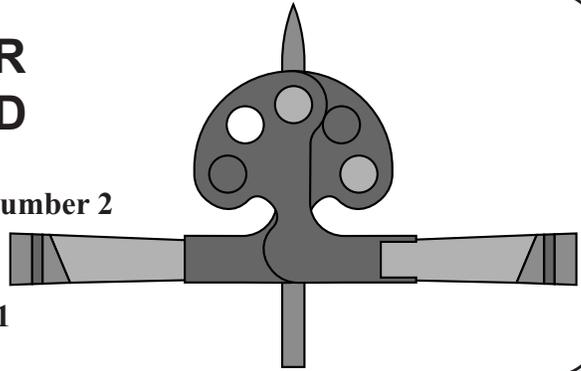
The **Dispatch** - ISSN 10457178 is published bimonthly by Studio G P.O. Box 745, Jonestown, PA 17038 0745 Email: T. Chuck Garman dispatch@tchuck.com
SUBSCRIPTION RATES: 1 year, \$25.00 which includes membership in the NASG. All subscriptions payable in U.S. funds. Postage is paid at Harrisburg, PA. Printed in the USA. All rights reserved. Postmaster: Send address change to Claude Demers at address above.

COVER: Top - Eastbound B&O #8, The Shenandoah, meets eastbound B&O freight, The Pittsburger, at New Castle Junction, PA. Photo by Dan Vandermause. Bottom is a B&O P1d Pacific created from an American Models Pacific. Photo by Brooks Stover

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The DEADLINES for articles are the 1st of February, April, June, August, October, December, for issues dated April, June, August, October, December, February, respectively.

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ADVERTISING RATES: Inquire regarding B&W or color rates. For additional information, contact the NASG DISPATCH Advertising Manager.



S Supplement: You may want to get the May *Railroad Model Craftsman* because Carstens is printing up an all-S supplement for this issue. It will be 16 pages

S RAIL MAIL

Not Plug and Play: Model railroading is not a plug and play hobby. Even taking the easiest scale and simplest approach there are still more than enough “problems” to turn off someone who doesn’t want to think for himself. Let’s face it, it’s no good trying to make it so easy that a lazy person with no desire to learn or develop skills can get along just fine. It’s the challenge that draws us ever deeper into this expensive time consuming, space eating and often frustrating hobby. We’re a particular type of people to be drawn to this. It’s not for everyone. That’s one reason that I can’t get excited about where the next generation of model railroaders will come from. There will always be folks like us who are driven to choose the “road less traveled.” We don’t have to be enticed to follow it, we search it out. I had all the things you folks have been saying are necessary to draw scalars to S. I left those things behind to pursue something different.

S has everything I want. One of those things is not having everything I want! It keeps me young and hungry, always pressing forward to the next goal. I don’t know about you, but if people didn’t look at me funny when I tell them I’m a model railroader, I’d find something to do that did. Now I’ve discovered the ultimate... model railroaders look at me funny when I tell them I’m in S! Model railroading is a journey, not a destination.

- Ed Kozlowsky, Sanford, Maine.

Another Way of Promoting the Scale Side: Is the NASG unable or unwilling to promote the scale only aspects of S, due to a large part of its membership being AF fans?

If that is the case, maybe the scale contingent needs to work outside the organization at promotion. One venue might be the 1/64 Scale SIG (Special Interest Group) under the NMRA. This is a current SIG, but the contact is listed as Richard Bendever with an invalid New Jersey address. This suggests that the SIG is again moribund.

long and have ads and content. The title will be *S Scale Primer*. If you happen to be at one of the shows that *RMC* attends you may be able to pick up the *Primer* free, but most of us should plan on picking up a May *RMC*. Thanks to our BOT and Alan Evans for this one.

www.NASG.org: Jim Bresnahan is now the official NASG Webmaster. Any updates can temporarily be sent to: jpbresnahan@cablemo.net.

If it could once more be resuscitated with an emphasis on promotion, it could provide an opportunity for a group to promote the scale aspect as they see fit. The NMRA should be a reasonable fit for this, especially as the organization is also trying to move away from the politics that discouraged many modelers in years past. Maybe the NASG would allow the group to also become a “Special Interest Group” with the bounds of the NASG. I’m really not advocating anybody leave the NASG as part of this.

Alternatively, those with strong feelings about how S should be promoted could form a group like the O Scale Kings who promote 2-rail O gauge railroading. In either case, I’d suggest foregoing the expenditure of energy on a newsletter, unless it only related to the business of the group, and perhaps follow the lead of the early NASG and use a regular column in *1/64 Modeling Guide* (if Bob Nalbhone agrees).

- Pieter E. Roos, Willimantic, CT. *Read the last paragraph of my editorial in the August 2010 Directory*
- Ed

Promoting the Scale Side: The promotion of S scale has been an on-going battle since I was in grade school, well over 40 years ago. It actually has worked but only at one tenth the speed it should have – kinda like our unemployment situation today. Does government (NASG and local clubs) or the private sector (publishers, suppliers and manufacturers) provide the money and the heavy lifting?

This weekend (Feb. 2011) the Houston S Gaugers will be running our scale modules at a local train show. We’re one of several clubs to participate – generally one club per scale. We’ve been doing this for 20 years. Unfortunately promotion means different things to different people and indeed to different geographical and population areas. The NASG has focused much of its efforts to larger train shows. This is generally a good idea because you can catch perhaps as many as 20,000 people in one place over a weekend. Other areas generally have much smaller shows where that ideas will have marginal coverage. Much more sparsely populated areas can only be reached by magazine, internet or direct mail.

- Bob Werre, Houston, TX

Continued on page 30

S OS from... TOWER

by
Jeff Madden



Note the new title for this column. When I first became editor I tried to think of clever title, but in haste I slammed down Jeff's Junction as it flowed with the two J's. But, I think I'll try a new title that reflects that the column is always about S. It'll still be my opinions (other than guest writers), but I think S TOWER speaks more to the topic. By the way OS is a telegraph key signal that means On Station for when a train passes.

LEADERSHIP:

What does the membership expect of our elected leaders and our volunteer committee members?

Well, let's start with the elected officials – the Board of Trustees (BOT according to the NASG Constitution). These are kind of my ideas, but let's remember that our BOT officers are volunteers even though elected.

That said, these positions do imply responsibility and leadership. These folks (Pres., VPs, Treasurer, Secretary), are responsible for our money from various sources – dues, ad revenue, Clearinghouse revenue, NASG car, etc., and they are responsible for determining the expenditures – *Dispatch*, NASG car and promotions being the main ones. They are also responsible for knowing our constitution and by-laws and abiding by the content therein. Of course, there are ways the members can change the constitution and by-laws, usually by a vote of the membership. Each BOT should review these and determine if changes are needed. The last time we published the constitution and by-laws was in the Directory in 2003, but they are available on the website to view under NASG Member Benefits.

Beyond the technical and monetary responsibilities, our BOT should be leaders regarding the membership and promotion of the scale of S outside of our organization. The upcoming RMC supplement mentioned on page 4 is an example of the latter. I feel the BOT should meet physically at least once or twice a year – they usually do. However, in today's world cell phones and email have made communication much more convenient and inexpensive beyond physically getting together. The new technologies can really help leadership when geographically separated as they are.

Committee members are appointed volunteers for the most part. These chairpersons when accepting a position also have responsibilities and duties regarding their particular committee – *Dispatch*, photographer, membership, projects, promotions, conventions, website, Clearing House, librarian, elections, Lionel NASG car and Standards.

As with the elected BOT these chairs need to be responsible for carrying out their duties in a timely fashion and for communicating to the BOT and members via the *Dispatch*, emails and the website. Over my 16 plus years as editor I have seen many BOT and committee members come and go. All have been pretty decent and given much personal time and effort to the NASG.

This little treatise explaining the obvious is not to be critical of any “volunteer”, either elected or coerced to be committee chairs. Naturally, with any volunteer organization things sometimes get out of whack due to various factors – jobs, illness, family calamities, and so on.

My feeling is that when things do get goofed up or out of sorts then the key to solving most situations is to “communicate” quickly and often. Sometimes we just forget or assume.

So keep these things in mind if you are nominated or asked to serve on a committee, for down the road we will surely need more “VOLUNTEERS.”

So think down the road. We should be getting some election ballots soon for VPs and and Secretary. These are elected “volunteers”. There might be some committee positions you'd like to chair or at least be a part of in the future. Offer to at least help out on these committees. Thankfully I have several “volunteers” who help out with the *Dispatch*. They are listed on page 3 plus others who contribute regularly such as Ed Kirstatter, Brooks Stover, Jerry Schnur, Monte Heppe and many others who send in photos or articles. These are appreciated.

And, should we revive the idea of adding two more VPs to the mix to help spread the “gospel” of S??? It was suggested several years ago to add two VPs, and then perhaps re-align the boundaries in order to cover more geographical territory, including parts of Canada. For example, we could have a Pacific coast VP (including west Canada), a North Central VP, a South Central VP, a Southeast VP and a Northeast VP (including eastern Canada). This would provide more leadership in geographically separated areas and help cover neglected areas - especially Canada and the southeast.

- Jeff

NEW PRODUCTS REPORT

By Jeff Madden

LIONEL LLC (www.lionel.com) Well, the 2011 Signature catalog arrived online on 3/31. Besides the Challenger and the U33Cs shown in the October issue, here are most of the new items. Five new road-names are listed for the Challenger though - Clinchfield, NP, WP, GN and SP&S.

Probably the item getting the most 'Buzz' is Lionel's new AF S gauge FasTrack which includes realistic looking track sections with realistic tie spacing and molded roadbed with ballast. The terminal straight sections lists for \$7.99. The straight and curve pieces are \$4.79 each. The curve radius is listed as 20 inch. The catalog says more straights and switches to come. At last! Not sure of code of rail?

Lionel has finally awoke and is offering an AF Circus train. The set includes a GP9, bay window caboose, flat with wagons, boxcar and streamlined combine and lists for \$549.00. For separate sale there's a searchlight car and a Moe and Joe Circus clown car for \$79.99 each. For \$69.99 each there also is another circus boxcar and a piggyback flat.

Not shown is a commemorative Docksider 0-6-0T and piggyback flat lettered American Flyer. The loco is \$109.99 and the flat is \$69.99.

Two PAs and a 3-pack of fluted passenger cars get the Texas Special treatment. The AA set sells for \$469.00 and the 3-pack is \$249.99.

New box and reefer cars are shown on page 7, \$69.99 each. Also shown are 5 new tank cars for the same price each. Note that one is the Buttermilk Bay Creamery car using tank containers on a flat car. The American Flyer (NE style) smoking caboose will sell for \$79.99. Two operating cars will be offered in new road names. The coal dump car will be lettered Northern Pacific and sell for \$89.99. The log dump car will be lettered Western Pacific and lists at \$84.99.

Four new scheme passenger cars are shown on page 7 and lists at \$79.99 each.

Finally there is a new accessory - a single Gilbert oil storage tank for \$79.99.



American Flyer | S GAUGE

AMERICAN FLYER FASTRACK

ALL NEW TOOLING!

AMERICAN FLYER FASTRACK
Lionel has taken their industry leading track system and created an all-new American Flyer FasTrack for S gauge!

- Real track appearance
- Easy connections for both power and tracks
- Molded roadbed and ballast for incredibly simple set up

C. 6-49854 Terminal Track \$7.99
D. 6-49852 Straight Track \$4.79
E. 6-49853 Curve Track \$4.79

NEW

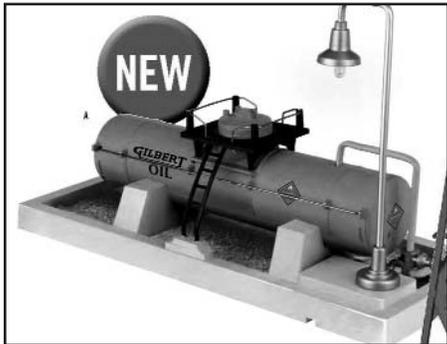
MORE TRACK SECTIONS AND SWITCHES TO COME!

20" RADIUS

12 PIECES PER CIRCLE

AF S-Gauge FasTrack

WHAT'S NEW



OPERATING CARS

- A. NORTHERN PACIFIC COAL BINPAC**
 - Die-cast metal tracks and operating coupler
 - Removes operating coupler, dumping bin, and coal bin for use with traditional American Flyer track
 - Length: 7 1/2"
 - #-NORCO 149.95
- B. WESTERN PACIFIC LOG BINPAC**
 - Die-cast metal tracks and operating coupler
 - Includes operating coupler, dumping bin, and log bin for use with traditional American Flyer track
 - MP decoration on top lid
 - Length: 7 1/2"
 - #-WOPCO 144.95



AMERICAN FLYER LOCOMOTIVES

American Flyer S Gauge enthusiasts have arguably lived in the shadow of their bigger O Gauge brothers for some time. That is about to change!





Welcome to Sacramento, California!

The dates of July 3-9, 2011, will see a cooperative convention effort between the NASG and the National Model Railroad Association (NMRA) in Sacramento, California. The combined NMRA and NASG annual convention, named "Extra 2011 West", or simply "X2011 West", will have extensive NASG participation for S enthusiasts of all persuasions, including Sn3 narrow gauge, S standard gauge and American Flyer-compatible.

For general convention information, use the red main menu bar above to go to the other pages of this website or use the "Search this website" tool.

Many S events are being planned to foster the family reunion feel of the traditional, smaller standalone S convention. This will be a full-featured S convention within the larger NMRA multi-scale community. We have arranged procedures so that registered S folks will have exclusive rights to register early for S events. Only after a specified cutoff date will non-S folks be able to register for an S event. Act promptly and you are in! Procrastinate and the S event might be sold out. (Note: This early date has passed! a/o 1/26/11)

Please note that the X2011 West website and this NASG S Scale Activities page are the best sources of ongoing information for this convention. As things progress, both will be updated frequently with new tours, clinics, events, news, advice and so forth.

Check back frequently or subscribe to the RSS news feed by clicking on the orange "RSS" button, located in the top right corner of any X2011 webpage. News and updates will automatically be sent to all subscribers of the RSS news feed.

All X2011 convention cars, banquet, layout tours, prototype tours, general interest tours and S activities may be ordered and purchased at the online X2011 Company Store.

Here's some news from Ed Loizeaux:

1. The Saturday night NASG banquet, named "Roundhouse Revue & BBQ Too", is nearly 50% sold out as I write this in mid-March. Since there are only a fixed

number of seats due to space restrictions, when the seats are sold out, they are sold out. I would suggest placing your reservation now via the X2011W online shopping cart if you are planning to attend.

http://www.x2011west.org/cart/index.php?main_page=product_info&cPath=10&products_id=55

2. Please note that one convention registrant can bring along a spouse, children, friends, etc. to the NASG banquet. It is not necessary for your wife or children to be registered for the convention. This is a great opportunity for everyone to enjoy a railroad-themed atmosphere with good trains, good food, good show and good camaraderie. Simply buy the desired number of tickets via the online shopping cart.

3. Note this is an "all-you-can-eat" dinner without limit. Thus, you can skip lunch and apply the savings toward your evening experience. Bring your appetite and a positive attitude. Western attire is totally acceptable. Got a 5-gallon hat? Bring it along!

4. The **Hubba-Hubba** organization of San Francisco will be producing the after-dinner musical revue and entertainment. Included in the show are the following acts: **Sister Kate** -- Wendy, Sarah & Ashley are San Francisco's own Cabaret Gypsies. With a wink & a smile, this trio of shimmying beauties combine burlesque & bellydance to create unforgettable acts guaranteed to tantalize & mesmerize! **Room for Cream** -- From far-off Sonomastan, and the shores of Old Eastbania, Jodi Waseca & J.D. Limelight captivate with laughs, music & dance. Don't be scared if a wild gorilla shows up, he's just there to play the accordion. **McPuzo & Trotsky** -- All the way from 1921, come these Troubadors for Troubled Times! Armed with an arsenal of timely tunes (hits like "Warren G. Harding is a Horse's Ass" & "Beef for India!") McPuzo & Trotsky are living proof that the banjo can indeed be used for Good! **Conductor Kingfish**, your Master of Ceremonies & Tour-Guide to Entertainment! **Zip the What-Is-It? Hairy Beast-Man** --and loyal stagehand!-- from **Barnum's Own Menagerie!** ...plus in the wings, Long-Suffering & Stalwart Stage-Manager, Comrade Tang!

5. If music and comedy are not your cup of tea, this is the opportunity of a lifetime to cavort amongst the railroad exhibits at the California State Railroad Museum without the hassle of crowds and long lines. You can count the rivets, measure the flanges, check the gauge and enjoy close railroad encounters of the third kind without interruption.

6. After the musical revue concludes, all the usual NASG contest awards, announcements, next year's convention preview, and so forth will commence.

7. The official NASG convention cars (yes, plural) with optional load are selling well. Please note that it

is not necessary to be registered for the convention in order to purchase these cars. Buy one or several and we will all be happy. Yes, there are AF-compatible versions as well as scale versions. Choose your wheels carefully for maximum happiness. Details are on the X2011W web site here:

<http://www.x2011west.org/nasg.html#convcars>

8. There are only a specified number of convention cars being produced. One version is close to being sold out. Once gone, it is gone! Your prompt order is advised if you want one of these cars on your layout or display shelf. Bill Roberts is happy to accept your personal check for mail orders. Or, you can order via the online shopping cart if you want to pick up your car at the convention. Either way works for us.

9. Just in case you have not heard of the 2011 annual NASG convention, the details are all here on the web site. Take a gander and you will like what you see.

<http://www.x2011west.org/nasg.html>

See you in "S"acramento.....Ed Loizeaux, Convention Co-Chairman

Clinics - So Far (subject to change)

Superb clinics are being planned for those who enjoy smaller flanges. Below is a list of presentations already arranged and more are being planned. There is no need to register in advance for clinics, except for certain "extra-fare" clinics, such as "Modeling with the Masters." Just walk in and grab a seat!

Please contact Ed Loizeaux at sscale@x2011west.org or Lee Johnson at sscale1@x2011west.org if you would like to conduct a clinic on any S-related topic.

- Adding Pullman Service to any Layout:
Jamie Bothwell
- Introduction to Car Ferries: Jamie Bothwell
- Painting Simple Backdrops: Jamie Bothwell
- Custom Modeling Using a Laser:
Dave Haehn
- Scratchbuilding Crossings in Any Scale:
Dick Karnes
- Making Pine Trees: John McKenzie
- Weathering Freight Cars: Roger Nulton
- The Inside Life of a Brass Importer:
Jettie Padgett
- Tales of a Hobby Shop Owner: Bill Winans

Contact Us / Questions?

Please read everything on the X2011 West website and these S scale pages first before asking S-related questions. Use the convenient "Search this website" tool to help find an answer to your question. If the answer to your question is nowhere to be found therein, then e-mail Ed Loizeaux at sscale@x2011west.org or Lee Johnson at sscale1@x2011west.org with your S-related question.

Non-S-related questions should be directed to the NMRA folks. See the X2011 "Committee" page for their contact information.

Reservations for all tours must be made through the X2011 Company Store online shopping cart.



Don Harper's Sn3 layout has been featured in nearly a half dozen magazine articles and fully occupies a spare bedroom.

Photo by Joe Visintine

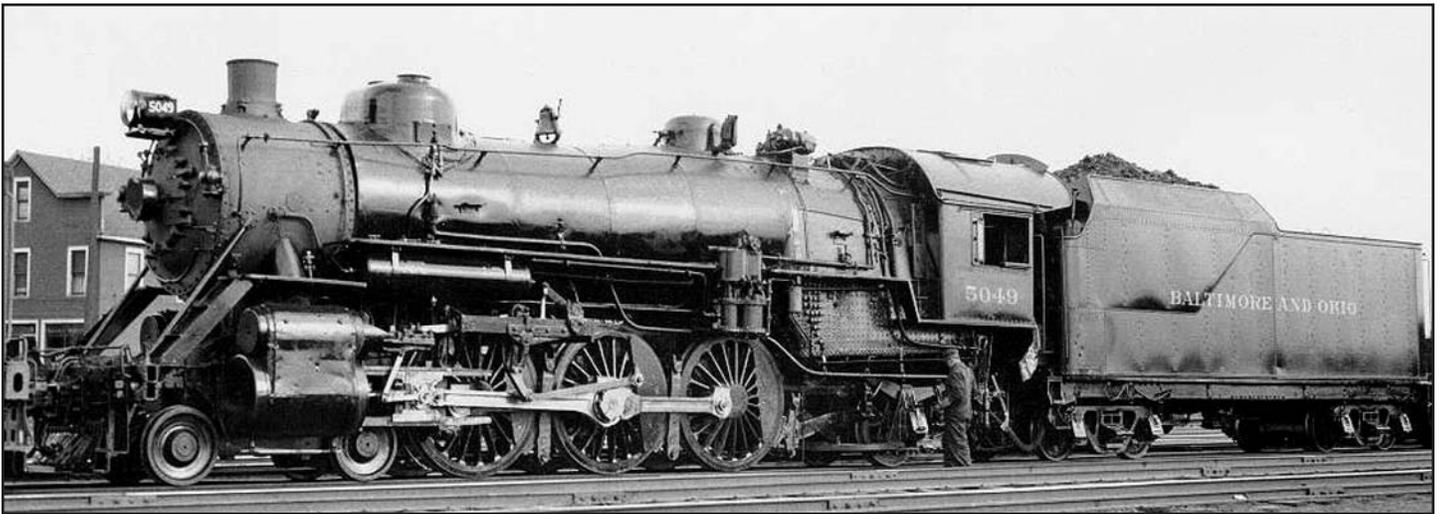


Figure 1 – This photo of B&O P1-d class Pacific #5049 was taken by Harold Vollrath in 1935 at Bloomington, IL

INTERPRETING A B&O P1-d PACIFIC

By Brooks Stover

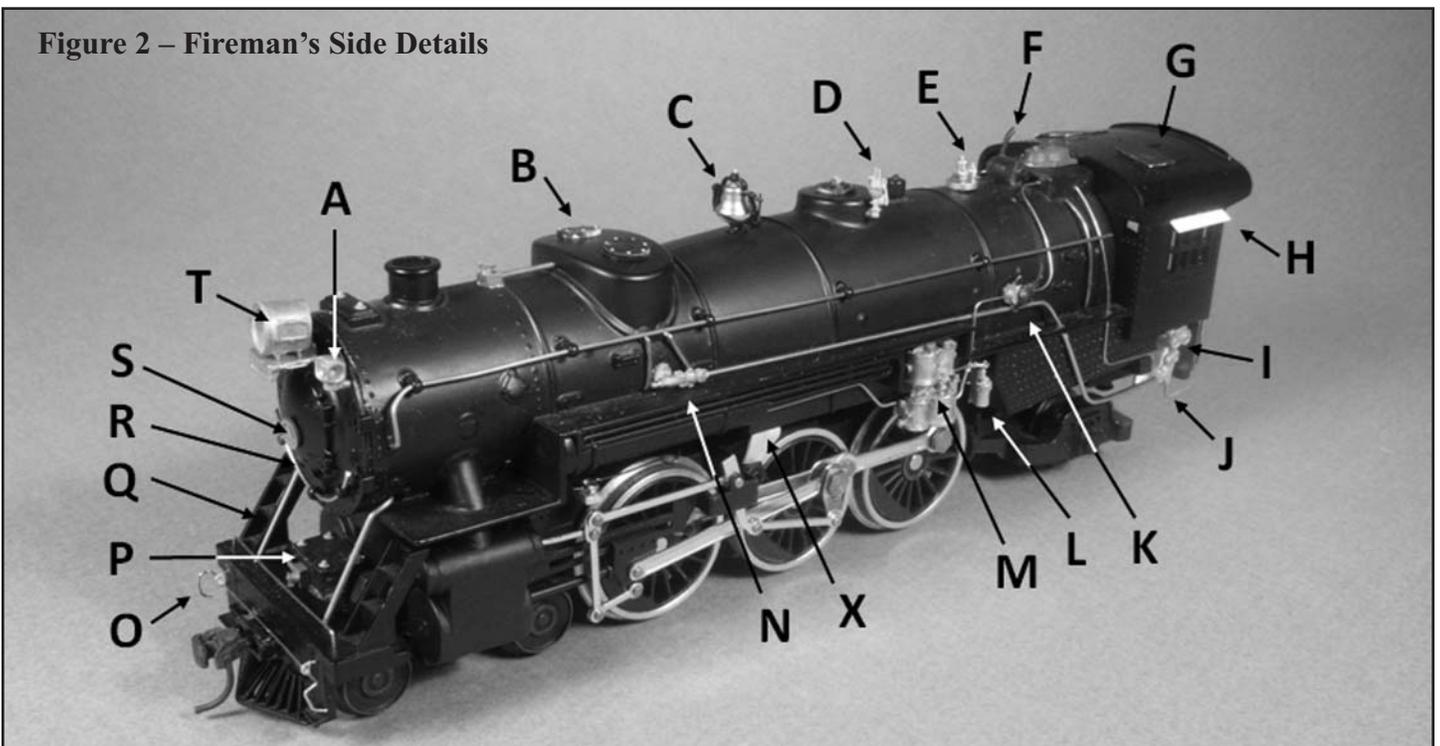
Photos by the author

For quite some time I've had one of American Models excellent running Pacifics sitting on the shelf under my layout. I purchased it many years ago but have never put it in service because it didn't really fit into the operating scheme of my Buffalo Creek & Gauley. While the BC&G interchanged with the B&O at the little town of Dundon, in the period I model (1958) B&O GP9s handled the interchange service and besides, the Pacific was primarily a passenger engine. So the Pacific has sat unused for nearly 10 years! I generally don't buy equipment unless I have an immediate use for it on my

layout, but I bought the Pacific because it reminded me so much of my beloved AF #28085 engine!

As the rest of my layout is nearing completion and I'm holding regular operating sessions, I've been able to turn my attention to some of those projects that have been lower on the priority list. One day it occurred to me that perhaps I could use the Pacific during an operating session by having it power a B&O railfan trip and make an excursion up the BC&G. There is no historical basis for such a fan trip although I do have in my collection a photo of one of the BC&G engines pulling three matched B&O coaches on the BC&G mainline. No one has been able to identify this event for sure. I've freelanced a couple of other trains in my operating sessions to add interest and I thought a fantrip would be a fun addition.

Figure 2 – Fireman's Side Details



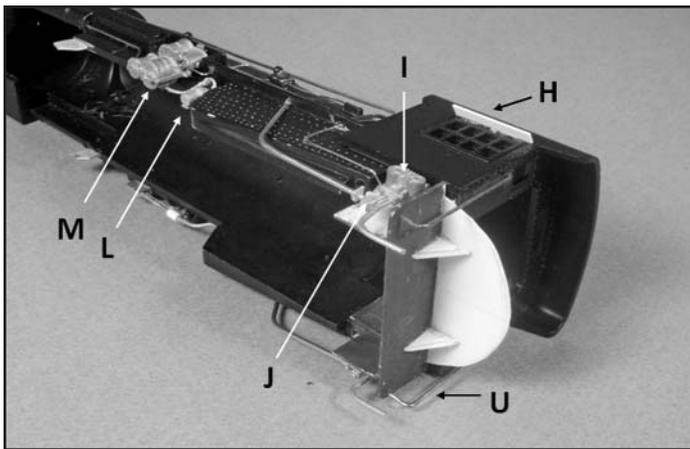


Figure 3 – Rear of Cab Detail

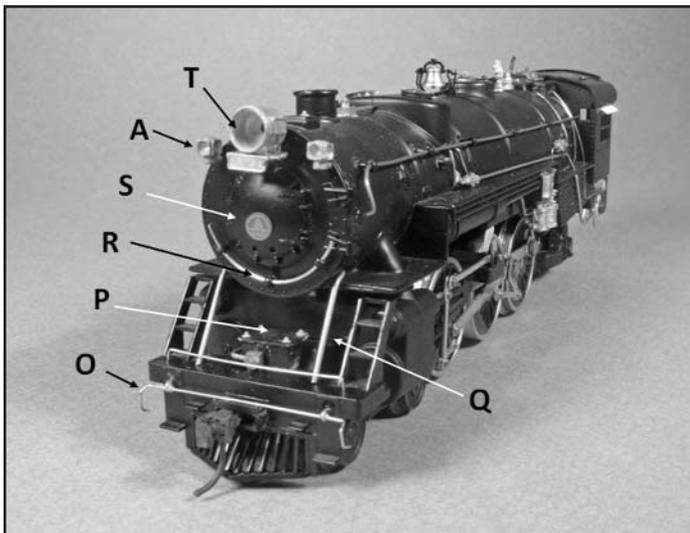


Figure 5 – Boiler Front Details

Selecting a Prototype

I researched the Pacifics that the B&O used in the area of West Virginia near the BC&G and found that their class P1-d Pacifics (Figure 1) were most often used on the short passenger trains common to the area in the final years of steam. The P1-d's had 74" drivers, very close to my AM hi-rail model's 75". I decided to represent one of the P1-d's and used prototype photos to determine the placement of appliances and other details. It's not an exact model, but 'good enough'! One notable change from the AM model is that P1-d's had their headlights mounted on the top of the boiler front, not in the center. The P1-d's, built between 1928 and 1938, were all converted from other classes of engines including some that were originally Mikados. There were 30 P1-d's built and they have been called "the old work-horse of the B&O passenger service". The P1-d's were often used in freight service during WWII and twenty were still in use in 1952 and the last was scrapped in 1957. The engines were numbered in the low 5000's, with numbers scattered between 5003 and 5094.

Detailing the Engine

While the AM Pacific is a smooth runner and has very nice see-through drivers and valve-gear detail and a see-through pilot, it lacks detail on its boiler. It comes with only two sanding pipes, a generic line at the rear of the boiler above the running board and the boiler hand rail on each side. Several years ago an "S" manufacturer produced castings to detail the engine but those kits are no longer available. Enter River Raisin Models. I contacted Dan Navarre at River Raisin (www.riverraisin.com) and ordered all the castings that would be needed to turn the engine into a more detailed model. Besides detail parts, River Raisin offers a full line of brass locomotives. The Table 1 lists the River

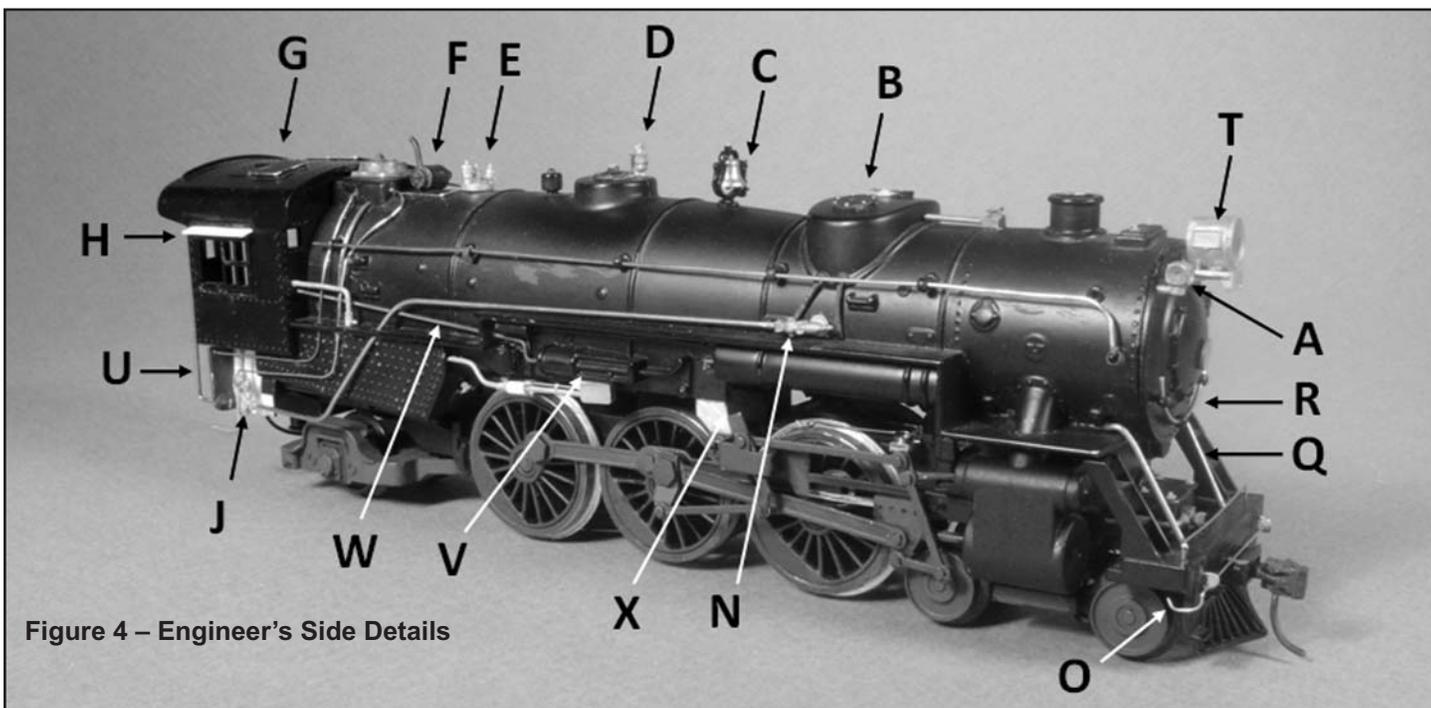


Figure 4 – Engineer's Side Details

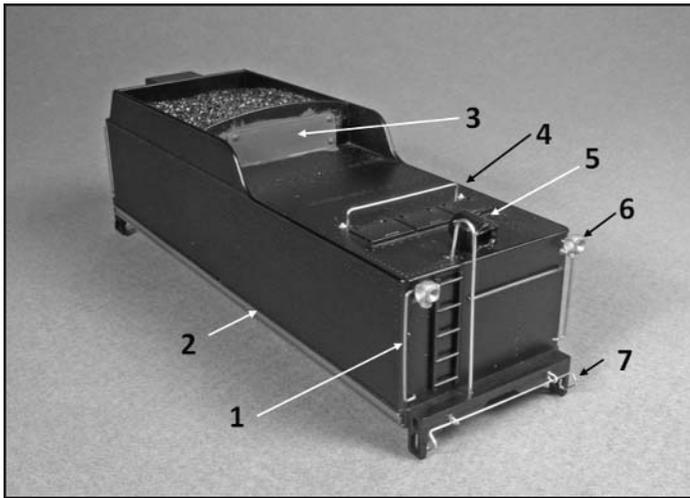


Figure 6 – Tender Details

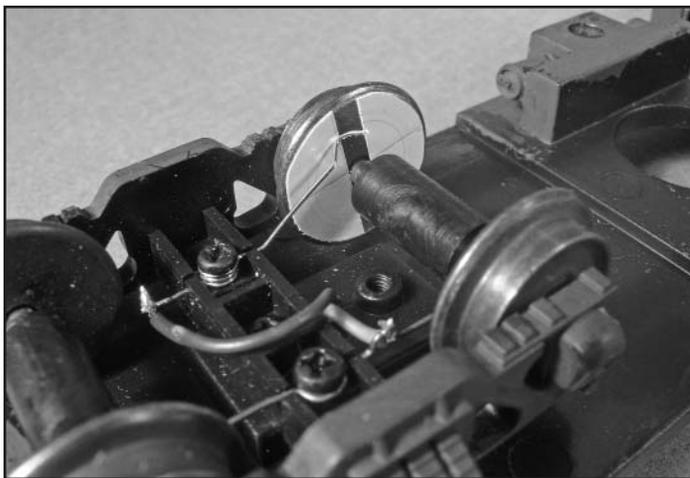


Figure 7 - Sound Cam Wiper Detail

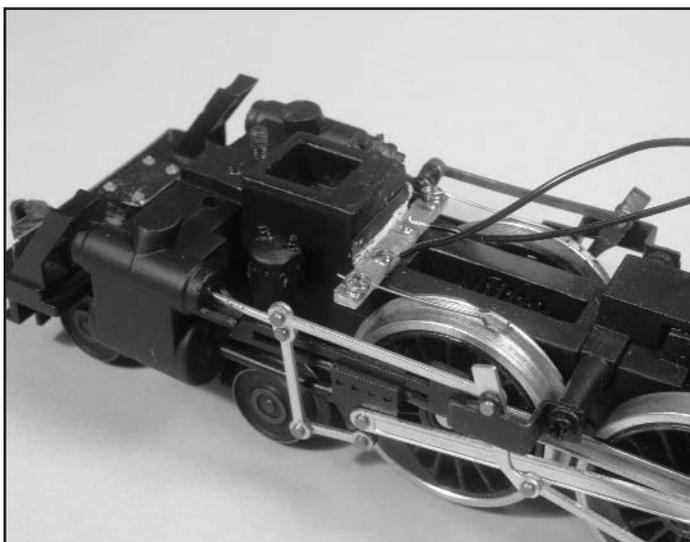


Figure 8 – Driver Pick-up Wiper Detail

Raisin parts that I used on this project.

The piping on the outside of the boiler is not too hard to do although I found considerable trial fitting is required. I used three different sizes of brass wire (.020", .032" and 3/64") to form the various pipes. The piping and components were attached by drilling appropriate holes in the boiler casting and securing with a drop of ACC. The good news is that the routing of piping is not critical as generally no two engines of even the same class were the identical. Changes over their lives were made as repairs and upgrades were required and different shops routed things differently. The general locations are important, of course, and the correct components need to be connected. Notice that much of the piping is at the rear of the boiler and enters the cab. The modeler can do additional research on the function of all the components.

Detailing the Fireman's Side

A major component on the fireman's side (Figure 2) is the air compressor (M) and the associated filter (L). The compressor on the AM model is a separate plastic casting and is easily removed. I installed the River Raising casting by notching out the back of it. You could also cut away the surface of the AM model in this area. The compressor governor (K) was made from a scrap box part. The piping running both directions from the compressor is .020 wire.

The other major components on this side are the injector (J) and the check valve (N). These are connected by a 3/64" wire and a .020 wire runs from the top of the injector to the top of the boiler. On the prototype a line runs from the bottom of the injector to the tender. I left the 'tail' on the casting in place to represent that line. Many model locomotives do not have representations of injectors as they are in a precarious location, easily damaged and have a tendency to foul the trailing trucks. But they contribute considerably to the transformation to a realistic model. To accommodate mounting the injectors, I fabricated surfaces under the cab from .030" styrene as shown in Figure 3. I also built a tender deck out of .020" styrene that nicely fills in the space between the engine and tender. My layout has AF radius track in several areas and so I checked all clearances carefully before finally assembling these parts. The stoker motor (I) is mounted above the injector on the fireman's side. All P1-d's had stokers.

A cab visor made of styrene (H) and a cab handrail of .032 wire (U) were installed. A small 'tab' (X) was added below the boiler to make it appear the valve gear was attached to something

Detailing the Engineer's Side

The injector (J) and check valve (N) installation is the same on this side (Figure 4) as the fireman's side. Photos suggest an additional line running down from the top of the boiler the turning back under the cab. I mod-

eled that with .020 wire. This side also got the cab visor (H), cab handrail (U), and valve gear tab (X). The other major component here is the power reverse (V). I chose not to cut out or grind off the power reverse on the engineer's side but simply added a little detail in this area. The River Raisin power reverse is beautiful and in hindsight I wish I had taken the effort to install it. The power reverse control rod (W) is made from .032 wire.

Detailing the Boiler Top

Sand dome hatches (B) were fabricated from scrap box parts. Cab roof hatches (G) made from .010 styrene were installed over the ones cast in. Details from River Raisin include the pop off valve assembly (E), generator (F), bell (C) and whistle (D).

Detailing the Boiler Front

The biggest change to the front of the boiler (Figure 5) was to move the headlight (T) to the top of the boiler front. Ultimately a number plate made on the computer was mounted under the light on the side of the headlight housing. The B&O emblem (S) shown in the photos is an S Helper item. On the finished model I used

an emblem made by reducing an 'O' scale decal. I replaced the plastic AM marker lights with brass units (A) for durability.

A boiler-front handrail (R) was made from .032 wire and three eyelets of the same type used on the handrails running along the boiler. The two boiler support braces (Q) were made from 3/64" wire and the cut lever (O) from .020 wire. I fabricated a prototypically correct train control relay equipment box (P) from styrene and NBW castings and installed a Kadee 802 coupler.

Detailing the Tender

Additions to the tender were rather minor. As shown in Figure 6, there are two vertical handrails (1) and a horizontal grabiron installed on the tender by AM. I added a strip of Plastruct 'C' channel (2) to the bottom edge of the tender body to simulate the frame and add some texture and I closed off the opening in the back of the coal bunker with styrene (3). I added a railing made of .020 wire near the water hatches (4) and aused the AM headlight as a tender mounted light (5). As with the boiler front, brass marker lamps (6) replaced the plastic AM units. I added a railing for the ladder (7) made from .032 wire and a cut lever (8) made of .020 wire. I later



Figures 10 & 12 – Only small details needed to be added to the tender. The tender coal pile was made by gluing loose coal over the molded-in pile. Paint was used to add 'lenses' to the marker lamps. The rear tender light is the original AM front light. Number boards on both the tender and headlights were printed on the computer. The River Raisin tool box is visible under the tender chassis.

added a greatly enlarged coal pile and mounted tool boxes under the tender.

Installing DCC and Sound

As it comes, the AM Pacific runs quite smoothly due to its large motor, nice drive train and large flywheel. However, I did replace the stock motor with a NWSL can motor to insure there was absolutely no unnecessary motor noise. This was just a matter of personal preference. I also replaced the stock worm gear with one of AM low speed gears since top speed is not an issue on my operations oriented layout. I removed all of the wiring from the tender except the leads from the trucks and removed the smoke unit as I would be installing a Soundtraxx Tsunami DCC sound decoder. There are already provisions for a 1 1/2" speaker in the floor of the tender and I installed a Soundtraxx speaker in this space.

I used a wheel on the right side of the rear tender truck as the sound cam (Figure 7). As it turns out, the diameter of the tender wheels are approximately half the diameter of the drivers. By using .005" styrene as a 'mask' with a gap on each side of the wheel, the wiper now makes contact with the tender wheel twice per revolution or four times for every revolution of the drivers. The brown cam wire from the decoder was soldered to this wiper. To make up for the loss of electrical pickup on that side of the engine, I installed wipers on the front

set of drivers using modified American Models wipers soldered to a small strip of circuit board (Figure 8). Power from these was routed back to the tender.

Painting and Weathering

I painted the engine and tender Floquil Engine Black and the drivers, valve gear and tender trucks Floquil Grimy Black. The smoke box was painted silver and heavily weathered. The side rods were painted silver and more lightly weathered. I painted the interior of the cab dark green. The decals are from various sources. I numbered the engine #5058, a number about in the middle of the range assigned to P1-d's. The engine was lightly weathered with pastel chalks to represent a restored engine that's been pulling a fan trip for several days. I installed a crew of Artista figures including a fireman standing on the rear deck. If I had it to do again I would strip all of the paint off the boiler before adding the details. The production paint on my model was a little thick and hides some of the rivet detail.

All in all, I'm very happy with the way this engine came out. While it took some considerable effort, the results look great. Accustomed to seeing only smaller Consolidations, crowds now eagerly gather along the BC&G mainline to watch the big Pacific come by whenever a fantrip is scheduled!

River Raisin Casting Parts List

Cross Compound Air Pump
Compressor Filter
SP Power Reverse
Nathan Injector (2)
Nathan Check Valves (2)
Twin Cylinder Stoker Engine
SP Safety Valve Cluster w/whistle
SP 5 chime whistle
Bell with Carriage
Pyle National Turbo Generator
Sunbeam Sheet Metal Headlight
Tender Tool Box (2)

Figure 11 – The engineer is in his seat and the fireman is cleaning some stray coal from the tender deck. Fortunately for him, the engine is equipped with a power stoker, the motor for which is visible under the cab above the injector. Both figures are from Artista.



Figure 9 – This photo of the fireman's side of the finished shows the weathered smoke box and light weathering on the sides of the locomotive and tender.



Figure 13 – Railroad workers give a wave as the big P1-d #5058 rolls past a maintenance shack. Engines of this class were in passenger service throughout the B&O system and because of their power were even used in freight service. Some of the 30 P1-d's that were built remained in service through 1952.



Figure 14 – The crew of #5058 is busy making final preparations before the recently restored P1-d rolls backward just a bit to take on water. The engine is pulling a fantrip which will traverse the BC&G's track to Widen and back on the author's Buffalo Creek & Gauley layout. *Photos by Brooks Stover*



AMERICAN MODELS K5 CONVERSION By Norm “Mick” Hinkle

The rumor was true! For years, word circulated among Pennsylvania Railroad aficionados that one of the two more powerful evolutions of the Standard of the World’s finest passenger locomotive had escaped the scrapper’s torch. And it was true. Mr. Jerry Schnur had arranged a tour of his S scale Cambria & Ohio River railroad, giving railfans full run of his road’s property. Clearing an overgrown path to a forgotten engine house and tugging on the doors, visiting fans were awed by the smiling face of Pennsy K5 5698. A Nixon tinsplate-to-scale conversion, she still bore her American Flyer motor, gearing and frame. Six wheel tender trucks – with a decided AF Hudson-like appearance – fed her juice.

She appeared ready to go until one looked closer at her running gear. The Nixon conversion had not aged well. Such was this conversion: One driver’s spokes had crumbled to nothing, leaving the tire captured by the drive rod alone. Other drivers and wheels wobbled on their axles; other centers were loose. In short, while the lady’s soul remained intact, she needed a heart, lung and underpinnings transplant.

What to do? American Models offered a scale streamlined K4 “Torpedo” at a bargain price giving benefits of a can motor and modern gearing ... could this be the donor? A call was placed to Mr. Ron Bashista and a few days later, a new scale K4 arrived. Comparisons were made and spacing appeared to be a near perfect

fit, with the American Models’ wheel placement much closer to prototype than the American Flyer spacing.

Would the AM smoke unit fit? Like a glove. The square-ish shape of the AM smoke unit filled the AF boiler’s circular void as if it had been designed to do just that (How about it, Ron?). Two Plastruct square tubes glued inside the AF cab housing formed mounting pads for the AM frame ears. Grinding away AF mounting screw tabs enabled the detailed AM valve gear to be used intact.

Replacing molded AF lumps with River Raisin’s excellent brass headlight, generator and air pump castings added detail. A brass ring capped the old AF stack. A sheet plastic tender floor with Plastruct square tubing as a center rib held the S Helper Service cabooses in place, providing eight wheel pickup. Kadee couplers – front and rear – clasped tight to passenger or freight loads. Scalecoat Brunswick green paint provided a smooth surface for Microscale HO decals. Floquil graphite covers the smokebox and firebox. Crushed real coal covered the tender’s load, while MV lenses in all markers and the headlight completed the conversion.

While the prototype suffered from a malady we might all enjoy (too much power), the model benefits from modern engineering and materials unavailable to S scalers until recently. Many thanks to the suppliers who make these products to benefit S scale. With their developments a 60+ year old American Flyer models can undoubtedly be revived.

Photo by Mick Hinkle



S-PIKES SouthWind's SP Cab Forward #4261 and RiverRaisin's SP Mikado #3265 pass each other on Bill Young's California style layout. *Photos this page by Joe Visintine*



American Models GP-35s in SP livery work through a typical northern California town.



Above - A B&O E-27a 2-8-0 switches Versailles, PA. The station is a MJB Models kit of the B&O depot at Dickerson, MD which was produced in S-scale through the efforts of Ed Sauers. Below - A B&O SW-9 works Glenwood Yard. An operating session brings structure and purpose to yard switching.

Photos by Dan Vandermause



S TRACKS

**Guest Author
Dan Vandermause**

Photos by the author

Where Am I and Why Am I Here?

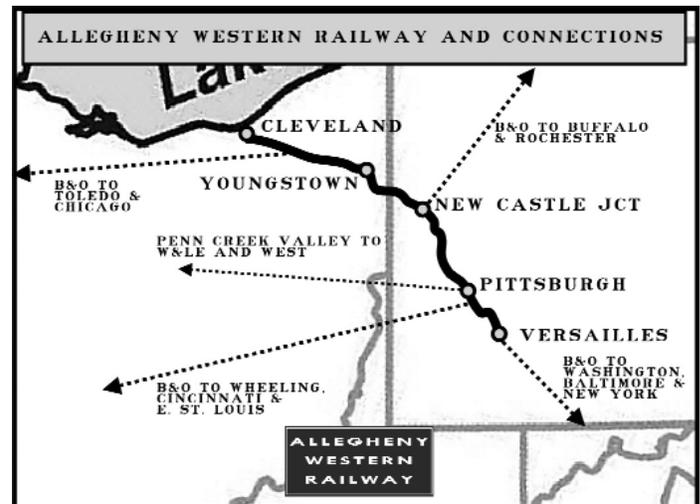
As a layout owner, we spend a lot of time thinking about what little slice of the world our layout represents, why it is there and how it relates to the larger railroad network. This thought process probably began when we first put pencil to paper to draw the track plan, and with each new piece of rolling stock, structure or square inch of scenery we add to our layout, we are building on the tale we have devised for our layout.

However, think about the operator who descends the basement stairs for the first time to “operate” your model railroad. He sees before him a sceniced (or possibly un-sceniced) model train layout, which is located, as far as he can tell, in a basement. How can we draw that operator into the world we have created, so that operating our model railroad makes as much sense to him as it does to us?

We need to provide that operator information about the world he is entering, and provide easily accessed visual cues to help answer the questions: “Where Am I and Why Am I Here? Although philosophers may have struggled with these questions over the centuries, luckily for us the answers are quite clear and easy to convey. Let’s start with the global perspective of “Where am I?”. Sam’s layout, the Penn Creek Valley, represents a fictionalized version of the Pittsburgh and West Virginia Railway. If you are a serious student of railroads, saying “Pittsburgh and West Virginia Railway” may be a sufficient explanation for you. But for most operators, it would be helpful to provide a little more detail. And, as is almost always the case in providing clear and concise communication, a picture can be worth a thousand words.

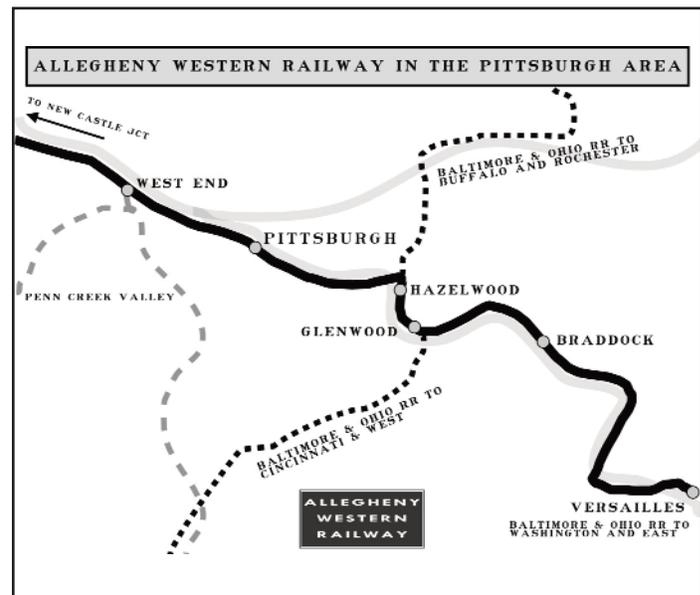
My own S-scale layout, the Allegheny Western Subdivision of the B&O Railroad, is also a fictionalized version of another Pittsburgh area railroad, the Pittsburgh and Lake Erie Railroad. In the real world, the B&O had trackage rights over the P&LE from the 1930’s until CSX eventually purchased the P&LE in 1992. In my fictionalized version, the P&LE is named the Allegheny Western Railway, and by 1966 (the year mod-

eled on my layout), the Allegheny Western has recently been acquired by the B&O. A more detailed description of the fictionalized history of the Allegheny Western can be found at my website at: www.AlleghenyWesternSub.com.



A new operator on my layout is given an employee timetable which includes a description of this fictionalized history, as well as an overall map of how the Allegheny Western fits into the larger railroad world:

This simplified map, which reflects the type of “routing map” many railroads produced, helps the new operator to understand what little slice of the world this layout represents. A second map included in the employee timetable shows the Pittsburgh area in more detail (this is the area modeled on my layout):



So, when an operator is working at, say, West End, this map can provide the geographic orientation to help the operator understand where he is, compared to that other operator on the other side of the basement, who is working in Braddock. And when a waybill says that a car is routed east over the B&O to Washington, D.C., the operator now has some idea of the geography involved in that move.

The employee timetable includes the above maps, and many other bits of information which can help the operator during an operating session on my layout. However, it is not always convenient or desirable to have operators pouring over their employee timetable in the middle of trying to move a train over the railroad. It is also helpful to provide the operator with instant visual cues about location and direction. For this kind of communication, fascia signs are very helpful.



Again, these types of visual cues help to answer the question "Where am I?" But how about some help with answering the question "Why am I Here?" In this case, adding schematic track diagrams to the fascia near each town on your layout can provide quick and valuable information to your operators:



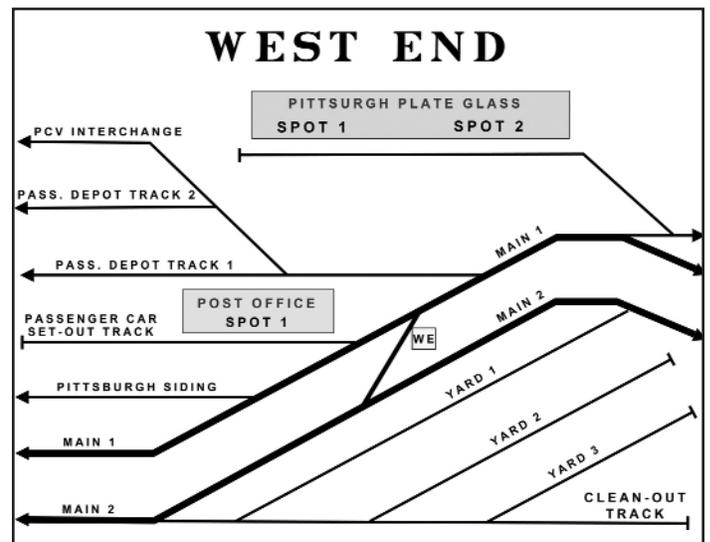
This is the track schematic for the town of West End. I created the track schematics for my layout using a Paint type program, but you can easily make these on your computer using the Draw toolbar on either MicroSoft Word or Excel. If you have never used the Draw tools before on Word or Excel, play around with the Draw toolbar options and you will quickly get the hang of adding track lines and text to your track schematic signs. The spreadsheet grid in Excel also provides a convenient grid to help in organizing your track schematic design.

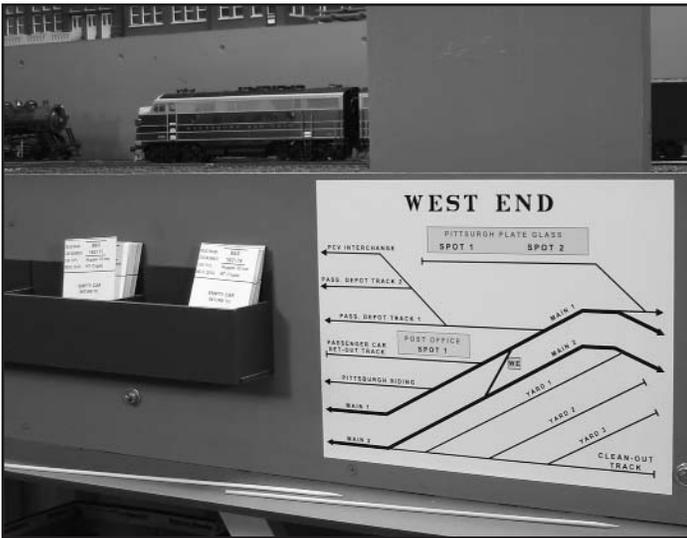
At each station on my layout, the operator will find these fascia signs:

In this case, the fascia sign clearly indicates that the operator is now located in Versailles, Pennsylvania. Another fascia sign tells the operator which direction is east and which direction is west. These signs are made using MicroSoft Excel spreadsheet software. These signs were made using the correct B&O station sign font, as produced by the B&O Historical Society. You can use similar fonts, like Times New Roman, which are probably available on your computer, or you can go to websites such as www.railfonts.com to purchase railroad-specific fonts for your fascia signs. To get you started, I have posted versions of my fascia sign spreadsheets in the files section of the S-scale Yahoo group, in a folder titled S Tracks-Operation.

Below is a larger version of the track schematic for West End:

I printed my fascia signs on glossy photo paper and attached them to the fascia using double-sided tape. If you have a Draw type program on your computer, you can even mock up your railroad's herald and print these out as fascia signs:





The track schematic sign conveys a lot of valuable information to an operator. First, note that every track has a number or a name. In an operating session at West End, when one crew member says to the other, “let’s drop those two covered hoppers on Yard Track 3”, the other operator knows exactly where Yard 3 is. The practice of naming or numbering each track is true to prototype – this is how the “big boys” communicate with each other in day-to-day train operations. You can have some fun in naming tracks, and you will be true to prototype. For example, S-scaler Ed Kirstatter, in an article in the Fourth Quarter 2010 issue of the B&O Historical Society magazine, described how B&O tracks at Akron Junction, Ohio were given names such as “Foot of Hill Track” and “Loop Line”.

Also note at the top of the track schematic that West End includes an interchange track with Sam Powell’s Penn Creek Valley (PCV). In the real world, West End, PA is the interchange location with the actual Pittsburgh and West Virginia Railway (the Penn Creek Valley) and the actual Pittsburgh and Lake Erie Railroad (the Allegheny Western Railway). As Sam will discuss in the next issue of the *Dispatch*, interchange tracks provide a good source of car movements for your operating sessions, and also provide the “beyond the basement” connection from your layout to the greater railroad network.

The track schematic sign conveys other bits of information to the operator which can make the operating session more realistic and more interesting. For example, note that Pittsburgh Plate Glass has two unloading spots at its plant. Spot 1 is for unloading of raw materials like glass sand, and Spot 2 is for loading finished product into boxcars. The waybill card (as described by Sam in the last issue) will specify at which Spot to place each inbound car. Extra switching moves may be required to get all cars at their proper Spots for loading/unloading.

Also note at the bottom of the track schematic sign that one of the yard tracks is designated as the Clean-Out Track. Certain customers may require, due to the clean or fragile nature of their product, that the railroad inspect the interior of the car and clean it out before spotting an empty car for loading. When an empty car is waybilled to be spotted at Pittsburgh Plate Glass for loading, the West End crew must first place the empty car at the Clean-Out Track. After a sufficient time to enable the mechanical department forces to inspect and clean the car, the car is then moved to Pittsburgh Plate Glass for loading. If you want to make the operation even more realistic, you can arrange for an occasional car to be rejected for its interior condition at the Clean-Out Track. This rejected car will have to be reassigned to a less-particular customer, and another empty will have to be waybilled in to West End to meet Pittsburgh Plate Glass’ needs.

These kinds of extra moves can really add interest to an operating session. Another example of an extra move that would add interest to your operating session would be moving an ice-bunker refrigerator car to an icing platform for pre-icing before loading, as well as back to the icing platform to have the ice load topped off after the reefer had been loaded. Another extra move would be moving a just-loaded car to the scale track for weighing before forwarding the car out of town.

Modeling these extra moves during your operating sessions can help to give the operators on your layout a good sense of the situations facing crews every day on the prototype. Now your model railroad not only looks like the prototype, but it even acts something like the prototype.

I would like to thank Sam for this opportunity to jump in and become a guest columnist and I hope you will find something useful here for your own layout. The better we are at communicating with our operators, the more smoothly our operating sessions will go, and the more fun everyone will have.

Finally, don’t forget that we have some excellent examples of S-scale model railroads which host regular operating sessions. I would recommend that you read Dick Karnes’ excellent article in the August 2010 issue of the NMRA Magazine entitled *So You Want to Run a Railroad*. Also, in the December 2009 issue of *Model Railroader*, Brooks Stover describes how he set up operations on his S-scale Buffalo Creek and Gauley Railroad. Finally, on page 32 of *Great Model Railroads 2011* is an illustration of the fascia signs Brooks Stover has installed on his layout.

More!

WHAT'S NEW



A company called Mount Blue Model Co. in Massachusetts has a couple versions of a New England farmhouse. The one shown here has a footprint of 3-1/2" x 9-3/8" and lists for \$64. A version with a small barn sells for \$99. The website is: www.mountbluemodelco.com. Doug Peck sells these via Port Lines Hobbies.



Round Roof Boxcar kit proposed : Smoky Mountain Model Works, Inc. is prepared to produce a kit for a Seaboard Air Line B7 boxcar. This car has a distinct round roof, but there are many differences between the SAL car and other railroads' round-roof cars. Jim King is requesting a full payment deposit of \$80 plus shipping, to determine interest to go ahead with the project. SMMW has successfully produced two kits for S scale locomotives and several freight cars For more information about the car, see www.trainweb.org/seaboard/SALRoundRoofBoxCarProject.htm or contact Jim directly: 35 Springwood Drive, Asheville, NC 28805 • (828) 777-5619 (9-5:30 EST) jimking3@charter.net



Shown are brass castings of an S scale US&S A-5 left hand CP valve (TP-508). These are castings that represent prototype switch machines available from the Irish Tracklayer for \$9.00. There are two other S scale versions - a right hand version (TP-509) and a US&S A-5 CPoint and CP valve (TP-510. These are also \$9.00 for one set of castings. The Irish Tracklayer is also producing an ATSF Merced, California tower as a laser kit for \$114.95 in S. He's mostly in O scale, but is doing a few things in S. Check out www.irishtracklayer.com



New from Pikesville Models!
S gauge High rail tank car. Wanzer Milk, a major milk producer in the mid-west from 1857 to 1967, refined and shipped milk throughout the central midwest states. This tanker proudly boasts "Wanzer on Milk is Like Sterling on Silver" available in two numbers. Limited production. see ad or visit Pikesville Models at americanflyertrains.com

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THE CLUB SANDWICH

By Dave Pool

The **Bristol S Gaugers (BSG)** members met at the home of Jan and Chet Brown, in Beverly, MA. for their meeting on January 9, 2011. Jan prepared a pulled pork lunch for those attending. Club members enjoyed a lengthy operating session on the host's layout, and Tom Robichaud hooked up his lap top computer to run trains through an interface over his cell phone. The members set up and operated their layout at the Big E Train Show (one of the country's biggest) held on Jan. 29/30, in West Springfield, MA. The club has attended this show for many years and this year they had a new lift gate bridge to display as part of the layout setup, and a power supply with T.M.C.C. to operate trains. The club has an archives committee that is cataloging historical data on the club past activities. Club members sent a sympathy card to Jim Babish on the recent death of his mother. The **BSG** planned to set up and operate their layout at the Greenberg Train Show, on March 26/27 in Wilmington, MA.

The **Connecticut S Gaugers (CSG)** set up and operated their modular layout at the Cheshire High School Ram Band Train show on March 6, 2011. The setup was coordinated by Steve Kutash, V.P. of the club, in charge of the module displays with the help of other members who bring modules to the layout displays. The club met in Madison, CT on February 19, 2011 at the Lutheran Church of Madison, CT. The guest speaker was Jack Swanberg who talked on New England RR operations, and presented a slide show on the subject. Following the program, members attending were invited to Bob's house to see his extensive layout and run trains (S gauge). The club made a contribution to the Church as a token thank you for use of their facility. The **CSG** is planning to meet at the home of Herb Cotter, in Weston, C. on April 23, 2011.

The **Pioneer Valley S Gaugers (PVSG)** recently met at the home of Dave Plourde, in East Lonmeadow, MA. Dave coordinates the club activities and Steve Allen plans and edits the club

newsletter.

The **Western N.Y. S Scale Association (WNYSSA)** held their February meeting at the Pegasus Restaurant in Hamburg, N.Y. Eight members attended the meeting. The club set up their 4 ft. x 8 ft. layout at the GSME Train Show on March 27, 2011 with the Rochester Area S Gaugers (RASG) in Batavia, N.Y. **WNYSSA** club members brought their trains to operate at the show. Paul Wachowicz hosted the club for a meeting on March 22, 2011, in North Tonawanda, NY.

The club set up and operated a layout at the TTOS Train Show at the Leonard Post on January 29/30, 2011, in Cheektowaga, NY. Don Webster and Gregg Mummert coordinated the club's attendance. Club members were considering the display of a layout at the October 30, 2011 TTOS Show, at the Leonard Post, Cheektowaga, N.Y.

The **Rochester Area S Gaugers Club (RASG)** set up and operated a display layout at the GSME Train Show on March 27, 2011 at the Batavia Downs, Batavia, NY. (see description above). The club has set up jointly with the WNYSSA club members at other train shows and it works well for both groups.. The club website includes a scrapbook and schedule of events page.

The **South Jersey S Gaugers (SJSJG)** recently celebrated their 21st anniversary, having their first meeting on July 29, 1989 in Cherry Hill, NJ at the home of Roy Hoffman. Bill Lane, Jr. and Roy Hoffman were the organizers of the club when it first started. A recent play trains event involved the club members trip to Pennsylvania to have lunch at Applebee's in Pottsville, and a visit to the home of Dennis Cannon to view and watch operation on his home layout. The host's home layout includes a lot of AF trains and some standard gauge trains all of which are operated. The layout includes an operating volcano and balloon rides.

The **Pittsburgh S Gaugers (PSG):** The club's hi-rail layout was sent to Lou Templeton's church to be set up for the Church's Holiday Festival, and then to be set up at the January Greenberg Train Show on January 22/23, 2011. It was decided to purchase two NASG club banners, which could be used at the train layout displays that the club attends. The February meeting of the club was held on the 25th of the month at the Kennedy First Alliance Church, near McKees Rocks, PA. A Coffee and Trains meeting of the club members was scheduled for Feb. 14, 2011 at the Harmerville Kings restaurant.

The **Baltimore Area AF Club (BAAFC)** club members met at the Marley Station Mall, Glen Burnie, MD hosted by Paul and Russ Love on January 15, 2011 for their monthly meeting. The hosts set up and maintain a model RR exhibit at the Mall. Ron Kolb was a co-founder of the club which now has 154 permanent members, and 12 prospective members. The **BAAFC** set up and operated their layout at the Great Scale Train Show held on Feb. 5/6 in Timonium, MD and the TTOS Train Show on March 13, 2011, in Earleigh Heights, MD. George Glover hosted the club members for their monthly meeting on March 19, and Bob and Sherry Fowler were scheduled to hold the May club meeting. The April 23 club meeting was scheduled to be held at the home of David Blair. Club members set up a layout at the Great Scale Model Train Show held on April 9/10, 2011 in Timonium, MD.

The **Chicagoland Assoc. of S Gaugers, Inc. (CASG)** set up and displaying their layout at both the High Wheeler Train Show (sponsored by the Fox Valley Div., NMRA) at Harper College on March 5/6, 2011, in Palatine, IL and the All-American RR Show (sponsored by the DuPage Div., NMRA), on March 28, 2011 at the field house of Lyons Township High School in LaGrange, IL. A recent issue of the club newsletter included a background on member Don Campion, who helped form the Chicago-land Area Narrow Gaugers (CLANG) group that constructed and display a narrow gauge portable layout at various train shows and other events. The club was planning on having a home meeting on April 15, 2011, but the location has not been set up as yet at press time.

He **Miami Valley S Gaugers (MVSG)** planned to set up and operate their layout at the Spring S-Spree to be at the Franklin County Fairgrounds, Hilliard, O. on April 29/30, 2011. A work session was held on Feb. 11, 2011 at the home of Larry Beam to work on the club's modules. Larry agreed to serve as chairperson for the Spring S Spree that they will host, to be held in May, 2012. Club member Jan Mason is coordinating the supply/purchase of club shirts. The club set up and operated their layout at the Springfield Train Show, held at the Clark County Fairgrounds, on April 3, 2011. The club recently received new member Rich Boehm from Mason, OH, who was introduced to the members at the last club meeting, held on March 5, at the home of Larry Beam in Dayton, OH.

The **Stateline S Gaugers (SLSG):** George and Ruthanne Sorensen hosted the March 20, 2011 club meeting. Several of the club members presented a program on S gauge at the Feb. 6, 2011 meeting of the local NMRA Chapter. This could be a fruitful way to get new members, and spread the word on our favorite scale of trains. The NMRA members are already interested in railroads, all we need to do is convince them about the best scale/size to model with, say some of the members. Vera Flood has produced a new 2-sided card to be handed out at train show displays (300 were printed). A good way to publicize the club. The club is scheduled to meet at the home of Joel Weber on April 17, 2011. Dick and Bernie Bird will host the club meeting on May 15, 2011. August 21, 2011 is the scheduled date for the club picnic to be held at the Olsen Forest Preserve, Roscoe, IL. The **SLSG** recently welcomed new member Cliff Stetter, from Poplar Grove, IL.

The **Central Ohio S Gaugers (COSG)** set up and operated a layout at the March 5/6 Great Train Expo Train Show held in the Lausche Building, Columbus, OH. The club also set up and operated their layout at the Buckeye Train Show, held at the Ohio Expo Center Lausche building on April 16, 2011. The club is scheduled to meet at the home of Dana Davis, in Ostrander, OH, on April 17 and at the home of Pat Nightengale in Marengo, OH on May 22, 2011. The club is hosting the 2011 Spring S Spree on April 29/30, 2011 at the Franklin County Fairgrounds, Hilliard, OH. The event will in-

clude two buildings with over 13,200 sq. ft. of dealer trading space, and a 4800 sq. ft. layout room with multi-operating displays (free to public). The Spree S car is a limited run Timken TOFC flat car with wooden deck. Art Lofton is the Spree chairman. There is a website for the event at: www.COSG.ORG

The **Southeastern Michigan S Gaugers (SMSG)** held their March 20, 2011 meeting at the home of Jerry Poniatowski in Wayne, MI, and their April 10 meeting at the home of David Held, in Imlay City, MI. The **SMSG** club has 41 paid up members as reported at the March 2010 club meeting by Earl Carlsen, club treasurer. Gordon Michael ably edits the club news via meeting notes. Bob Stelmach is the president of the club at the present time. Gordy Michael is Secretary; Earl Carlsen, Treasurer; and Tom Hess, Bill Bartlam and Tom Hawley serve as Members-at-Large on the club Board of Directors. Gaylord Gill is scheduled to host the club members for the May, 2011 meeting, and Dave Campbell to host in June, 2011. The club planned to bring and operate their display layout at the Spring S Spree, on April 29/30, 2011, in Columbus, OH.

The **Northern Ohio S Scalers (NOSS)** held their March 27, 2011 meeting at the home of Jack Greenwald, Seville, OH. Club member Mike Warman hosted the meeting, held at the location of an O gauge layout enjoyed by those members attending the meeting. Jack Sudimak coordinates the club activity. Ed Kirstatter has taken over the club newsletter duties. The most recent issue of the newsletter has a masthead designed by Ed, who recently celebrated a significant birthday. Some of the club members attend the meetings of the NMRA Division IV, which welcomes guests. A recent meeting of the group was held at the Church of the Redeemer, in Westlake, OH. The **NOSS** displayed and operated a layout at the NMRA Div. 5 Railfest 2011 Train Show, held at the Community College, Kirtland, OH, on March 19/20, 2011. A recent newsletter issue describing the box cab diesel loco as used by the Central RR of New Jersey at their Bronx, N.Y. terminal (No. 1000). The article describes the Bronx terminal and has a photo of the round freight terminal building and track around it. The terminal could only be reached by car floats across the

Hudson river. A manufacturer called Great American Locomotion has recently offered for sale a partial kit (body only) of the small diesel described.

In mid-March the Northern Ohio S Scalers (NOSS) set up their 45 foot switching layout. This NMRA Mid-Central Region Division 5 show is a very large show, and this is our second year at the show. There were N scale, HO and AF hi-rail layouts there also. Dealers were from all over. A lot of people came and took pictures and videos. We got some of the same old comments, "I didn't know there was this much available in S Scale." This was often followed by, "If I didn't have so much in HO, I would switch." Or, "If I was starting over, I would go to S Scale. It is the perfect size." This latter info was supplied by John Henning

The **AF S Gaugers of the St. Louis Area (AFSGSLA):** On Jan. 8, 2011 the Macy's Holiday Layout was shut down and secured for the Summer months. The club recently established an e-mail address, at: afsgsla@sbcglobal.net Club member David Stevens coordinates the club meeting locations and times, to help reduce the work load of Gary Mueller and Moe Berk. The club has been planning for the **Fall S Fest, to be held on Oct. 28 through Oct. 30, 2011**, to be held at the Airport Marriott. This is the same location as the Fest sponsored by the club in 2007.

The **Inland Empire S Gaugers Assoc. of the Pacific Northwest (IESGAPN)** has a club newsletter edited by Vic Chervan, Jr. who recently took over the editorship from Jon Kettner. Ron Tilton hosted the club members for their January 15, 2011 meeting in Spokane, WA. Vic Cherven does an excellent job of editing and distributing the club news letter, *The S Train Rail*. The March 12, 2011 meeting of the club was held at the Corur d'Alene Casino in Worley, ID which was the site of the club's annual banquet

The **IESGAPN** club is implementing the awarding of a door prize at each meeting to one of those members attending the meeting. Members must attend two meetings after they win a door prize before they can be considered for another door prize, to insure that all the members have a reasonable chance to win. The club brought, set up, and operated their layout at the February 20, 2011 train meet at the Spokane

City College. Club member Bob Bowen has a building in St. Maries, ID that can be used by the club to set up and work on their layout. Recent club meeting discussions have been connected with the “direction of activity and interest of the club members” regarding scale or high rail and AF train interest. Vic Cherven, Jr., has made a survey of the member’s interests so as to help in planning club activities and events that will interest the most club member participation. Doug Sassman was scheduled to host the April, 2011 club meeting in Chewelah, ID.

John Eichmann coordinator of the **Rocky Mountain High Railers (RMHR)** sent out an announcement recently describing the activities of the club and the status of their portable layout. Their newest layout has two levels, the lower of the two is 30 inches above the floor to accommodate the view of children, and the upper level is 5.5 inches higher than the lower level. The layout includes a twelve track double-ended yard for train assembly/storage. The layout was featured in the recent issue of the Dispatch magazine, with several large color photos, and a track plan.. The club layout is named Lookout Junction III, and it requires a space of 22.5 ft. x 26 ft., about the same as the previous layout. At the present time the next display of the layout

is scheduled for July 7-10, 2011 at the National Train Show in Sacramento, CA. Several members have reserved rooms at the Super 8 Motel, at the Sacramento, CA. airport, and plan to bring the layout to Sacramento, CA to display and operate it there

The **Southern Calif. S Gaugers (SCSG)** held a module construction session at the home of Bob Palmquist, in Garden Grove, CA on Jan. 29, 2011. The club planned to hold their own train show/swap meet at the First United Methodist Church, in El Monte, CA. on March 5, 2011. Fred Ruby and Don Stratton were coordinating the event. The club plans to host the 2013 S Fest West show, and plans are formulating for the event.

The **Bay Area S Scalers (BASS)** held a meeting at the home of Lee Johnson, on January 15, 2011, in Walnut Creek, CA.. Graham Henry edits the club newsletter, called the *Bass Waybill* Graham is planning to move to smaller quarters, and will have to down-size his model RR activities. Operation on the Swayzee Pacific Transportation Co. layout at the Sept. 18, 2010 meeting of the club, hosted by Graham, may be the last at the present location, in Berkeley, CA. Lee Johnson coordinates the club meetings and events.

Lee wrote articles describing recently available S products from various S suppliers that appeared in recent newsletter issues. **The club is the official host group for the joint NASG/NMRA convention to be held on July3-9, 2011, in Sacramento, CA. (Sacto, as abbreviated).** Ed Loizeaux is chairman for the host group, and he has sought committee members to help organize and carry on the event. Reservations for those planning on attending are to be made via the Internet, according to news releases in a recent issue of this magazine. The club layout was displayed at the O Scale National S West Convention in Santa Clara, CA. on January 27/29, 2011 Jim Sweeney hosted the March 19, 2011 meeting of the club in SanRafael, CA. Club members Bill Young and Joe Visintine edited an article on Bill’s S scale Southern Pacific RR in a recent issue of the *S Gaugian* magazine..

Your club column editor wishes to thank those club members and clubs that submit information on the activities and events that you organize and attend. Please submit information and newsletters for the column to David Pool, 11 Bittersweet Trail, Wilton, CT.,06897-3902 or e-mail at: ndpool@juno.com.



On Feb. 19 the Houston S Gaugers set up a 16' x 22' layout at the Stafford Convention Center in Houston, Texas. There was a fantastic turnout.
Photo by Peter Vanvliet



Above is a scene on the modular layout of the South Jersey S Gaugers at the World's Greatest Hobby Show in Edison, NJ this year. Photo by Don Thompson

Below In mid-March the Northern Ohio S Scalpers (NOSS) set up their 45 foot switching layout. This NMRA Mid-Central Region Division 5 show is a very large show, and this is their second year at the show. Right is Gary Ippolito's SW-1 on the module. below shows an overall view with the club banner below. Photos by John Henning



Club Potpourri of Club Activities



Another closeup of the South Jersey S Gaugers layout. Note, that this Falcon Metalworks factory is a Walther's HO Cornerstone building that was modified. *Photo by Don Thompson*

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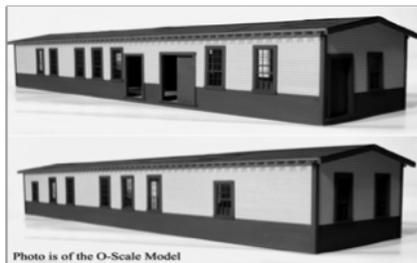


Photo is of the O-Scale Model

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EXTRA BOARD

ROGER NULTON NEW MMR
Congratulations go out to Roger Nulton who becomes MMR #445 in the NMRA. MMR stands for Master Model Railroader. The announcement was in the March *NMRA Magazine*. This is our second S Scale MMR in just a few months. Brooks Stover was the other. For those unfamiliar with the NMRA's MMR programs - well, it's sort of like the "Master Merit Badge" after going up the ladder. It's a great way to promote S scale.
- Jeff

RS-1 DEBUTS AT TEXAS SHOW

On Feb. 19 the Houston S Gaugers set up a 16' x 22' layout at the Stafford Convention Center in Houston, Texas. There was a fantastic turnout. This was also the public debut of my recently completed PRR RS-1 engine. The engine ran for about 6 hours without any problems. The North Yard chassis performed flawlessly. I didn't turn up the volume of the sound decoder loud enough, so people had to get up close to be able to hear it. That's a lesson learned for the next show.

- Peter Vanvliet

O/S WEST

O/S West was held again this year at its usual venue, the Hyatt Regency, Santa Clara (CA) On January 27-29. Several S scale vendors were there including Paul Vaughn with his novel uncoupling device; Howard and Judy Sheffield of Howard's Trains with their extensive stock of S products and Dan Navarre of River Raisin Models with his beautiful line of SP steam locos and parts. The Bay Area S Scalers' (BASS) portable layout was one of several layouts on display at the show. There were several clinics, including one by S scaler Jerry Porter who described the principles behind his design of Dave Scott's layout based on the SP

in the Dunsmuir-Mount Shasta area of northern California. Highlights included layout tours to Arden Goehring's Union Pacific Sherman Pass layout in Lodi and to Ed Loizeaux's NYC layout in Los Altos.

- Dick Karnes

NEW CLUB IN WEST MICHIGAN

The West Michigan S Gaugers is a newly formed group of fellows located in and around the Grand Rapids area in Michigan. The group meets on the second Thursday of the month at 6:30 p.m. Meeting locations vary between members' homes and restaurants. Meeting locations are determined by interest at the current meeting. Most of the fellows operate or collect American Flyer trains, but all aspects of S gauge are welcome. The group gathers to discuss the operation, repair, collecting and swapping of S gauge model trains. Sharing information on upcoming events, publications and dvds helps us keep up on what is happening in the "train world." Currently we are assembling from parts a 322 as sort of an ongoing clinic. The group has decided to explore the arena of module building so we can bring S gauge to the local train shows and other venues as we discover them. Our goal is to have a group of modules together for the December holidays. For more information contact: Charlie Hancock, 269-543-4397, charles_hancock@comcast.net or Don Keil, 616-304-8244, dkeil@yahoo.com.
- Charles Hancock

MILWAUKEE RIB-SIDE

A while back I started a campaign on the internet to see who would be interested in a Milwaukee Road 40' rib-side boxcar via Smoky Mountain Model Works in resin cast. We fell short before as I think Jim needs 80 plus cars to make it a go. Tom Baker and I are trying again. Perhaps there are some *Dispatch* readers who aren't on the Yahoo lists that could help support the project. I have no affiliation with

SMMW, but am willing to help coordinate reservations. Contact me with your interest.

- Bud Rindfleisch BlackDiamondRR@gmail.com

S IN NON-S PUBLICATIONS

- Feb. 2011 *NMRA Magazine*: Article by Dick Karnes on *Layout Height Consideration* with photos of Dick's layout throughout.

- March 2011 *NMRA Magazine*, Short article featuring Roger Nulton as the newest Master Model Railroader recipient.

- May 2011 *RMC*, The S Gauge pullout section.

- May 2011 *CTT*, Two page article by Ron Schlicht (BSG) on *How to Model a Country Fair*, plus a 1-pager on Flyer's No. 23791 "Cow on Track" accessory.

2010 FALL S FEST

According to the recent CASG newsletter registrations at the 2010 Fall S Fest in Tinley Park totaled 497 with a net attendance of 705. Registrations were from 32 states and Canada. The EJ&E cars sold were 125 of #1 and 106 of #2. The CASG is starting a waiting list for those that might want a car. Of course, they haven't been delivered yet because of the overseas mess. All 235 tables were sold plus 6 booths. Banquet attendance was 122.

S-TIPS

Frog Juicers: I am using hex juicers from Fast Tracks (the turnout jig folks). I like the feel of manual ground throws for turnouts and am using them in every case except for those few turnouts difficult to reach. Using the frog juicers for ground throws is the way to go as you don't have to worry about mechanically coupling a micro or slide switch to the ground throw. On the other hand, if you are using Tortoises or blue points, etc., which have built in frog polarity control, the frog juicers are superfluous.

- Walt Jopke



Calendar



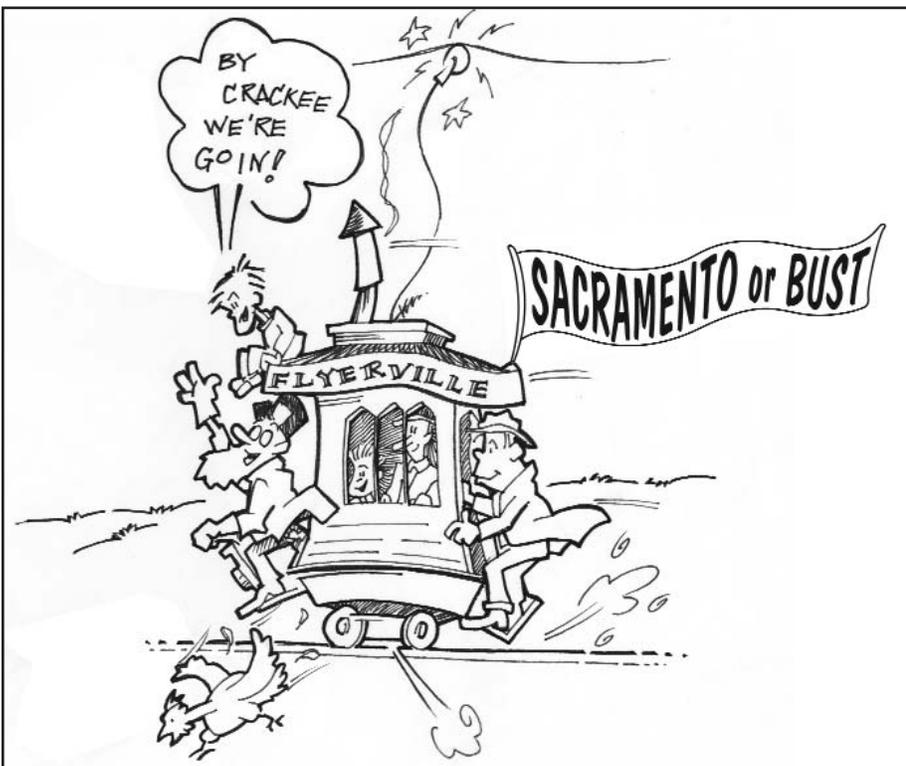
Dan Navarre and Lee Johnson commiserate at the O/S West Meet back in January in Santa Clara, Ca. Photo by Dick Karnes

July 3-9, 2011: X2011 NASG (NMRA) Convention, Sacramento, California. This will be a joint convention with the NMRA. www.x2011west.org.

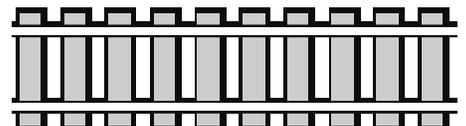
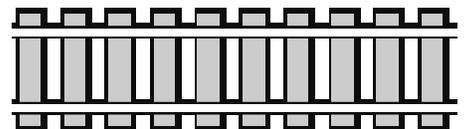
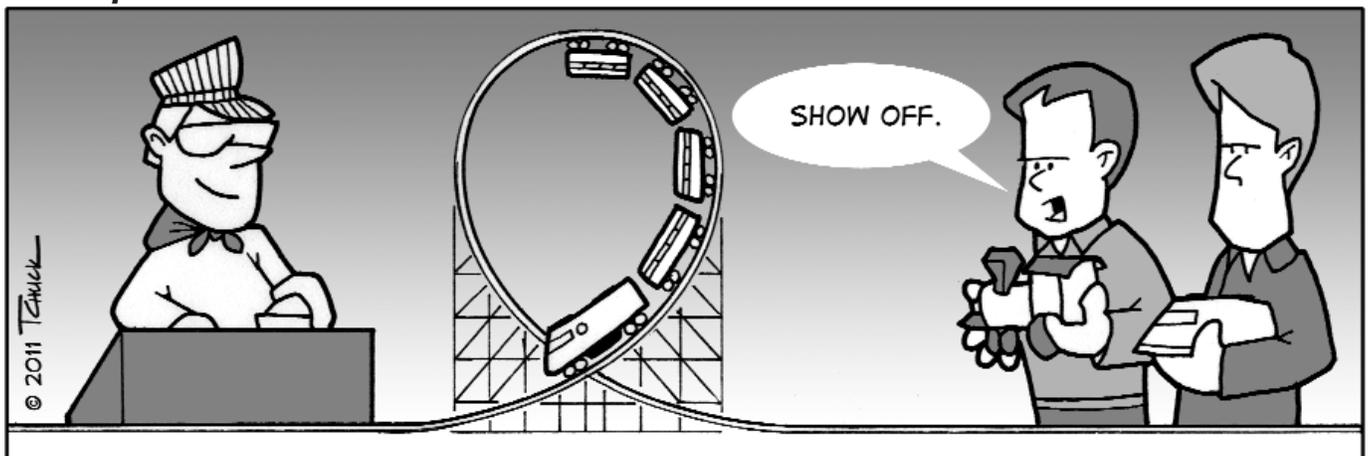
Oct. 28-30, 2011: 36th annual Fall S Fest, St. Louis airport Marriott Hotel. Rooms \$79 per night. Contacts: afsgsla@sbcglobal.net or try - www.trainweb.org/afsgsla.

Aug. 7-12, 2012: NASG Annual Convention, Chattanooga, TN. Details TBA.

Bids open for NASG Annual Conventions 2013 and beyond. contact Lee Johnson, 2472 Lariat Ln., Walnut Creek, CA 94596-6635. 925-943-1590 - email: leemax@jps.net. Lee will pass on the convention info to the new chairman as soon as one is selected. Ed.



Asleep At The Switch



RailMail continued from page 4

Rubber Gauger Sees the Light: I moved to Maryland some time ago where ½ of the basement included a G, standard gauge, and O combined layout. Now, I am adding an S separate layout, with room still remaining for a tentative N and Z small layout. Multi-gauging seems to be my weakness. I am now Ambassador #1210 in the World's Greatest Hobby at age 51. Now, I am convinced that S gauge is the most practical and all-inclusive way to go. It is just the best size overall for every reason I can think of. So, from here on S will be my way of life. The other trains will undoubtedly be sold to make room for the S. I joined the NASG a long time ago, and after getting lost in “rubber gauging” I’m

back to concentrate on S. Note: The TCA Museum in nearby Strasburg, PA has been expanding their S layout.

- Floyd LaBarre, Jr., Rising Sun, MD

Dues Increase: There is a ready market of potential members for the NASG in the membership ranks of the S clubs. Many do not belong and do not believe that the NASG membership is worthwhile as they have their own local friends and activities without belonging to the NASG. Raising dues does not make it easier to get these people to join the NASG. Perhaps the BOT could have found alternate revenues or cut some services.

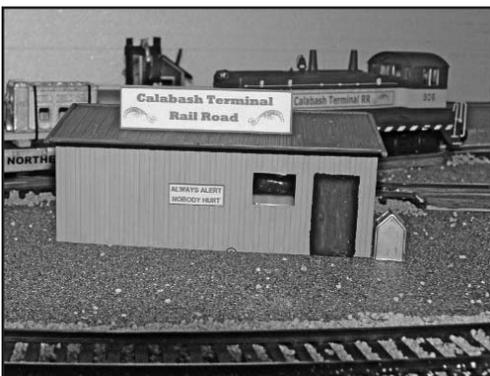
- Dave Pool, Wilton, CT

Calabash Terminal Update



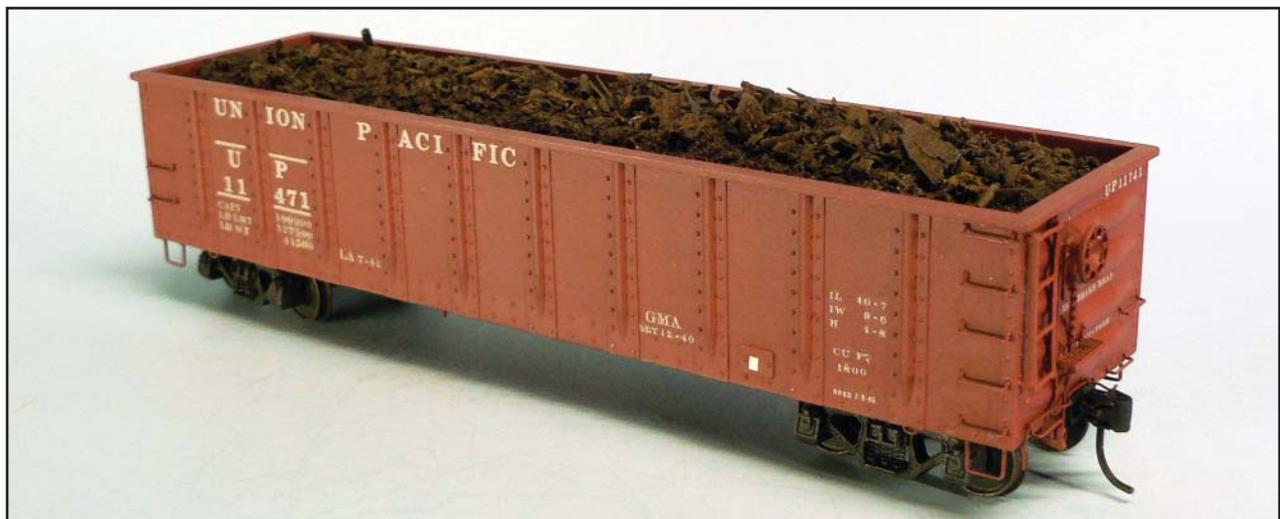
Some new scenes on the Calabash Terminal Railroad, a 2' x 8' portable switching layout, built by Monte Heppe as described back in the October 2010 issue. American Models track and switches are used.

Photos by Monte Heppe





S Helper Service has offered special loaded flatcars using their GSC 53'6" 70 Ton car. I got this one that has two Stuart light tanks loaded on it, Corgi made these. This time they have darkened the decks down before mounting these with a cast plastic chock blocks for which holes had been drilled for them then cemented to deck along with the tank treads. No tie downs were added. These early WWII tanks just don't belong on this Southern Pacific flatcar built 5-54 well after this war and with Hydra-Cushion under frames. The only way we can rationalize these tanks with both British and US marking is to say they are on their way to a Museum somewhere. Even though they are small tanks they appear to not be made to S scale but they do check out OK to the dimensions listed for these in a U.S. Military Vehicles Field Guide by David Doyle. Pictures from back then show three of these small tanks on a single flat car but closely spaced. These were the first American tank to carry a name named by the British after famous Confederate Cavalry General Jeb Stuart. These American designed and built light tanks were ground combat battle tested in WWII by the British 8th Army in North African desert before the US entered the war. These were the M3 an improvement over the M2A3's and M2A4's under development since 1939. They were armed with a high velocity 37mm gun using antitank ammunition. They also sported two Browning .30 caliber machine guns one in bow and other on the turret. The motors mounted in the rear were modified air-cooled radial gasoline aircraft type engines driving track cross-drive and transmission in the bow. *Copy and photos this page by Edwin Kirstatter*



Not a real scale looking conversion to S scale is this Union Pacific high side gondola for carrying scrap metals converted from a Marx O-27 car. It still has those very large looking side stakes and corrugations on the ends. It scales out to be a 40'6" long by 10'6" wide car in S scale and those sides are 7ft high. Not really too bad looking if you are not too picky. It's only about a foot too short compared to a like UP car. If you detail it up with many parts it starts to look pretty good. An Ace Flyer to scale conversion under frame was used it had to be shortened a little then it had complete brake piping and rigging added. I made up from Balsa wood a dummy load to cement metal shavings from a machine shop on it to look like a load of scrap metal going to the steel mills for re-melting. I first painted it a rust color. I also did an Erie car like this with the same load. SHS 70 Ton plain bearing trucks were used on these cars.

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